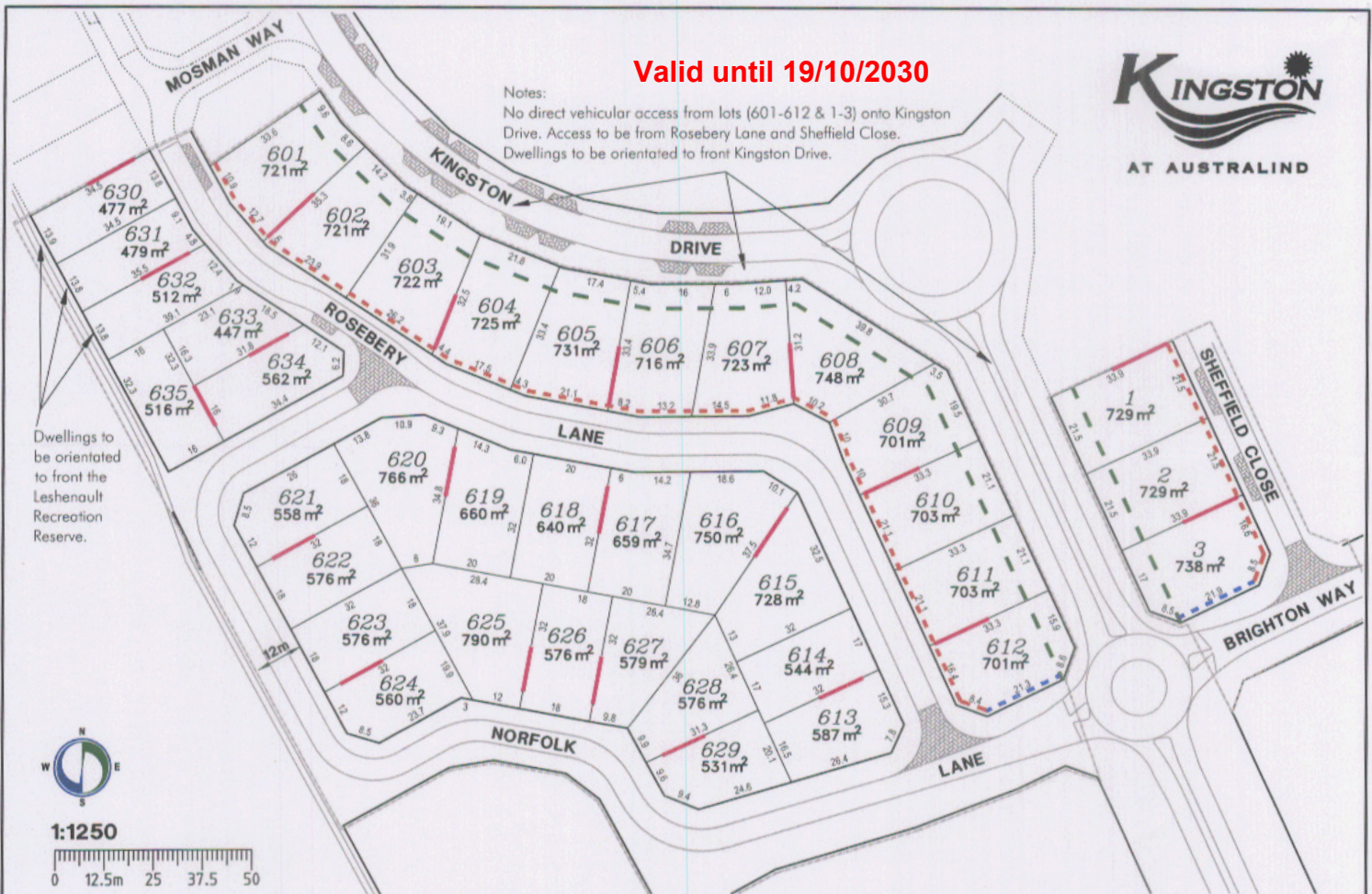


Valid until 19/10/2030

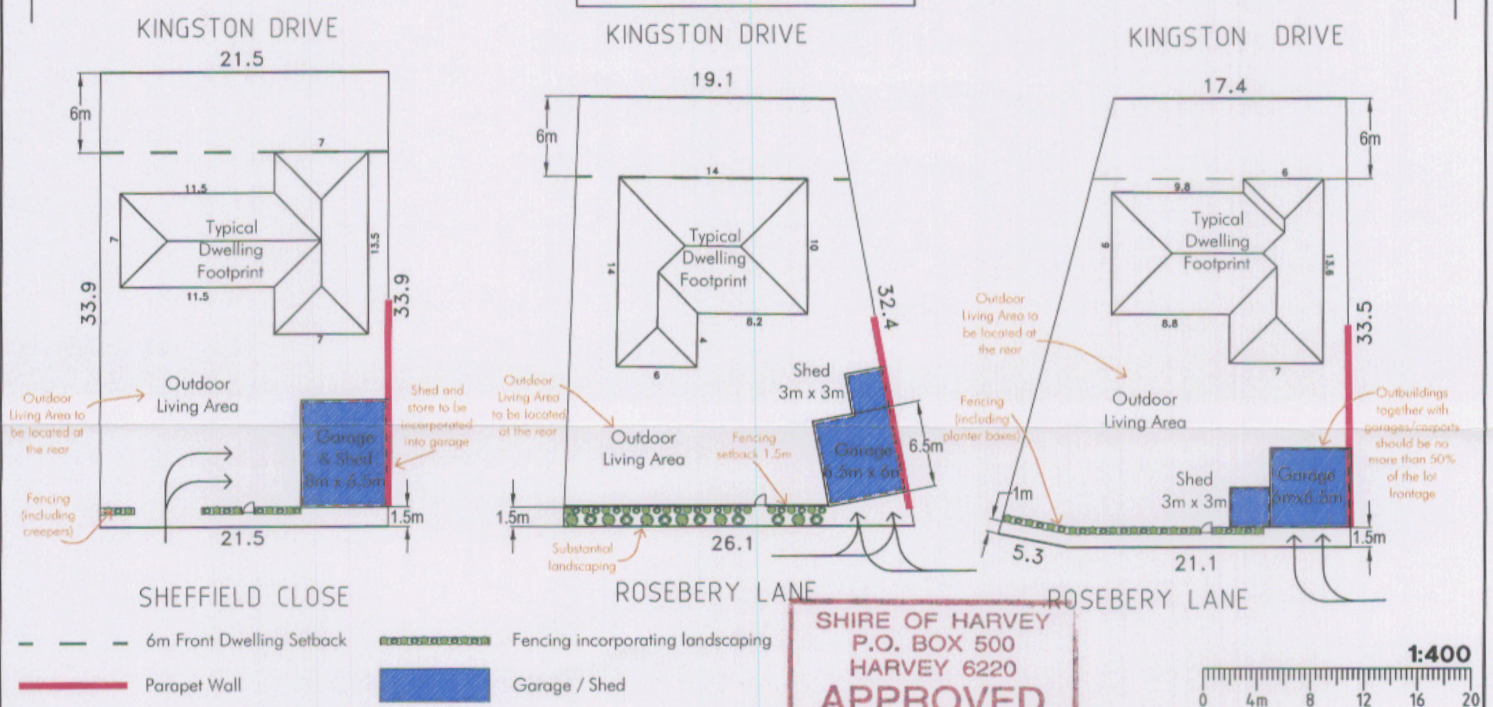
Notes:
No direct vehicular access from lots (601-612 & 1-3) onto Kingston Drive. Access to be from Rosebery Lane and Sheffield Close.
Dwellings to be orientated to front Kingston Drive.



- 6m Front Dwelling Setback (on dual/triple road frontage lots)
- 1.5m Rear Laneway Setback (on dual/triple road frontage lots) for garages, carports, sheds and outbuildings.
- 1.5m Side Laneway Setback on triple road frontage lots for sheds and outbuildings only.

605 731m² No lots to be resubdivided.
Location of parapet walls.

Indicative Lot Layouts



DATE	NO.	ACTIVITY - REVISION DESCRIPTION	DES	CHKD	APPR	DATE	NO.	ACTIVITY - REVISION DESCRIPTION	DES	CHKD	APPR
02/08/08	1	Initial submission	LVE	SB	LVE	10/01/06	1	Final submission	LVE	SB	LVE
PROJECT: Stage 2B Subdivision, Kingston Estate						Project Number: P97045-027					
DRAWING TITLE: Detailed Area Plan						Drawing Number: P312-SK67					
PRINCIPAL: Australian Vanguard Limited						Revision: A					
This drawing has been prepared in accordance with the Cardno BSD Quality Management System. It remains the property of Cardno BSD Pty. Ltd. and shall not be used without permission. This drawing shall be preliminary only and not for construction and/or approval.						Date: 31.01.06					
						Sheet: 1 of 1					

**PLANNING DESIGN CONTROLS FOR DETAILED AREA PLAN (DAP)
FOR STAGE 2B SUBDIVISION – KINGSTON ESTATE**

FILE NOTE: 50000119

1. APPLICATION OF THE DAP

The Detailed Area Plan (DAP) shall apply to lots within Stage 2B Subdivision of the Kingston Estate plus 3 lots in adjacent Stage 9A. For ease of reference, the DAP only refers to Stage 2B. The additional 3 lots (ie. lots 1, 2 and 3 on the DAP) are included in the same subdivision approval as Stage 2B that imposed the requirement for the DAP (WAPC Ref: 125486). All applicable lots are shown on the attached DAP.

The DAP is supported by planning design controls that address development issues relevant to the Stage 2B Subdivision. The planning design controls mainly focus on dual or triple road frontage lots on Kingston Drive in accordance with the subdivision approval.

2. OBJECTIVES AND IMPLEMENTATION OF THE DAP

The key objectives of the DAP (including planning design controls) is to prohibit any resubdivision of larger lots (700m² or greater) and to address the design issues applicable to dual or triple road frontage lots facing onto Kingston Drive. The DAP requires dwellings to be orientated to front Kingston Drive with access being restricted to the rear laneways (ie. Roseberry Lane and Sheffield Close).

The DAP addresses design treatments along the rear and side laneways to ensure that development (ie. rear and side fences, sheds, garages and carports) does not detract from the streetscape or reduce the visual amenity for dwellings located on the opposite side of the laneways. Typical elevations and indicative lots layouts have been included in the DAP to illustrate how lots on Kingston Drive can be developed in compliance with the planning design controls.

Residential density for Stage 2B is in the process of being downgraded from R25 to R20. Until this process is finalised, the DAP ensures that lots cannot be resubdivided in accordance with the R25 density code in Table 1 of the WAPC Residential Design Codes 2002 (also referred to as the R Codes).

The DAP also includes lots not fronting onto Kingston Drive in terms of identifying appropriate boundaries for parapet walls. In addition, the DAP requires dwellings to be orientated to front the Leshenault Recreation Reserve where lots directly adjoin the reserve.

The DAP fulfils Conditions 1 and 20 of the WAPC subdivision approval of Lots 9001 and 9003 (WAPC Ref: 125846). Implementation of the DAP and the planning design controls will be the responsibility of the Shire of Harvey. Building licences will need to be consistent with the DAP and the controls.

3. RELATIONSHIP TO THE ODP

The ODP prescribes the major structural elements of the Kingston Estate, including land use, road layout and residential density. The DAP is consistent with the design and intent of the ODP, but also addresses specific issues relevant to the Stage 2B subdivision, such as the design of development along rear and side laneways and the potential for resubdivision.

4. DAP DESIGN

The DAP illustrates many of the following planning design controls and provides indicative lot layouts to demonstrate how lots on Kingston Drive can be developed in accordance with the controls. The DAP is also supported by typical street elevations.

5. PLANNING DESIGN CONTROLS

Development on all lots included in the DAP shall be in accordance with the *Residential Design Codes 2002* (R Codes) and the Shire of Harvey District Planning Scheme No. 1 unless varied by the following planning design controls. Justification and objectives for the following design controls are provided in the attached explanatory notes.

Reference to "rear laneways" in the following planning design controls is a reference to Roseberry Lane and Sheffield Close, whilst "side laneways" is referring to Norfolk Lane and Brighton Way as shown on the DAP.

The planning design controls are broken down into three sections: controls that are relevant to dual or triple road frontage lots on Kingston Drive, controls that are relevant to lots with no frontage to Kingston Drive and controls that are relevant to all lots in the DAP.

The following planning design controls are only relevant to dual or triple road frontage lots on Kingston Drive (Lots 601 to 612 and Lots 1 to 3):

5.1 Building Orientation

- 5.1.1 Lots shall have dwellings orientated to address Kingston Drive as their primary frontage. Notwithstanding this, the design of residential developments on these lots will also consider and contribute to the streetscape of rear/side laneways and provide amenity to lots located on the opposite side of the rear/side laneways.

5.2 Regular Front Setbacks

- 5.2.1 Dwellings shall be setback a minimum of 6 metres from Kingston Drive. Averaging of the front setback is not permitted.

5.3 Access

- 5.3.1 No vehicular access onto Kingston Drive shall be permitted from lots fronting this road. Any garages and/or carports are to be developed to be accessible from rear laneways only and not side laneways.

5.4 Garages and Carports

- 5.4.1 Garages and carports shall be setback a minimum of 1.5m from rear laneway lot boundaries.
- 5.4.2 The maximum width/length of carports and garages fronting a laneway shall be 7m (including walls). Any carports located parallel to rear laneways shall be screened by fencing in accordance with Clause 5.10.

5.5 Rear Outdoor Living Areas

- 5.5.1** Lots with frontages onto Kingston Drive shall have outdoor living areas oriented towards the rear laneways.

5.6 Sheds and Outbuildings

- 5.6.1** Sheds and outbuildings on Kingston Drive shall be setback a minimum of 1.5 metres from rear/side laneway lot boundaries. Sheds and outbuildings should be integrated within the garages and carports thereby minimising building width along the laneway frontages. The setback of outbuildings and sheds should not only take into consideration the impact on the streetscape of the laneways but also make efficient use of the lot.
- 5.6.2** Sheds and outbuildings shall be designed and located so as not to be directly visible from any laneway or street. That is, the shed and outbuilding should be located behind a screening fence, or preferably, integrated with a garage structure.

5.7 Parapet Walls

- 5.7.1** Parapet walls are only permitted in the locations shown on the DAP and may only be used for the purposes of garages, sheds and outbuildings. The DAP shows parapet walls on a portion of the boundary commencing from the common boundaries of Lots 611 and 612 and Lots 2 and 3 and on the northern boundary of Lot 1. The length of the parapet wall shall be no more than half of the respective lot depth measured from the rear laneway boundary.

5.8 Minimum Building Width Along Rear Laneway Boundaries

- 5.8.1** The total width or length of sheds, outbuildings, carports and garages shall not exceed 50% of the lot width measured at the rear setback line as specified in Clauses 5.4.1 and 5.6.1. The only exception to this control is Lot 608, in which case a maximum total width of buildings relative to lot width of 70% shall be permitted.

5.9 Building Appearance to Rear and Side Laneways

- 5.9.1** Development shall not comprise any large areas of blank wall (including fencing, sheds and garages) without any major openings or other design features to reduce building bulk and impact when viewed from any street or laneway.
- 5.9.2** Outbuildings, sheds, garages and carports shall be roofed to a pitch that complements the main dwelling.
- 5.9.3** Outbuildings, sheds, garages and carports shall be constructed with materials and colours that are compatible with the main dwelling.
- 5.9.4** Garages and carports facing rear laneways shall be equipped with a garage door so as to provide screening of internal storage areas.

5.10 Fencing

- 5.10.1 Rear and side fencing shall be no higher than 1.8 metres along any laneway boundary, with any fencing above 1.2m being constructed with visually permeable materials (as defined under the Residential Design Codes).
- 5.10.2 Fencing shall be setback a minimum of 1.5m from the rear and side laneway boundaries and the setback area shall be substantially landscaped. Alternatively, fencing may be constructed closer to the laneway boundaries providing landscaping is incorporated within the fencing design as illustrated by indicative lots layouts on the DAP and typical elevations (see below).
- 5.10.3 Any fencing along the Kingston Drive frontage lots shall be no higher than 1.2m and may be constructed with solid non permeable materials.

5.11 Services

- 5.11.1 No goods or materials shall be stored so as to be visible from rear and side laneways (including carports). All goods shall be stored within garages, sheds and outbuildings, equipped with doors and/or appropriately screened by fencing.
- 5.11.2 Any service fittings, fixtures, clothes drying areas and rubbish bin storage areas shall not be visible from any street or laneway and are to be appropriately screened.

The following design controls are only relevant to Lots 630 to 632:

5.12 Building Orientation

- 5.12.1 Dwellings on Lots 630, 631 and 632 shall be orientated to front the Leshenault Recreation Reserve located directly to the west of these lots. Notwithstanding this, the design of residential development on these lots will also consider and contribute to the streetscape of Rosebery Lane and provide amenity to lots located on the opposite side of the lane.

5.13 Parapet Walls

- 5.13.1 Parapet walls are only permitted in the locations shown on the DAP and may only be used for the purposes of garages, sheds and outbuildings. The DAP shows parapet walls on a portion of the common boundary of Lots 631 and 632 and on the northern boundary of Lot 630. The length of the parapet wall shall be no more than half of the respective lot depth measured from the laneway boundary.

The following design control is relevant to Lots 613 to 629 and Lots 633 to 635:

5.14 Parapet Walls

- 5.14.1 Parapet walls are only permitted in the locations shown on the DAP and may only be used for the purpose of garages. The DAP shows parapet walls on a portion of the boundary commencing from the common boundaries of Lots 613 and 614. The length of the parapet wall shall be no more than half of the respective lot depth measured from the laneway boundary. Lot 626 has a parapet wall shown on two boundaries but

only one parapet wall boundary shall be used. The parapet wall on Norfolk Lane for Lot 634 may be used for the purpose of a garage, shed and/or outbuilding as this is effectively the rear boundary of Lot 634.

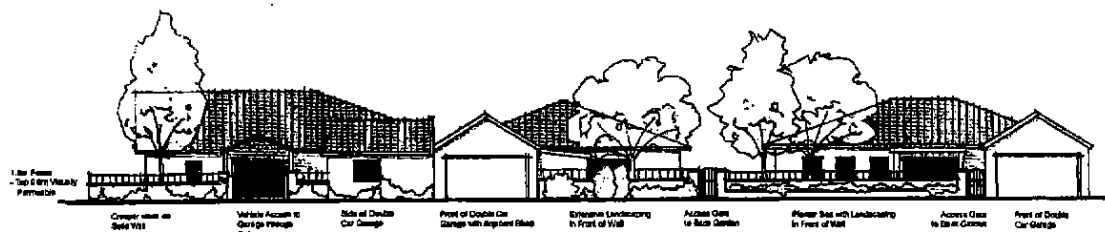
The following design controls are relevant to all lots included in the DAP:

5.15 No Resubdivision of Lots

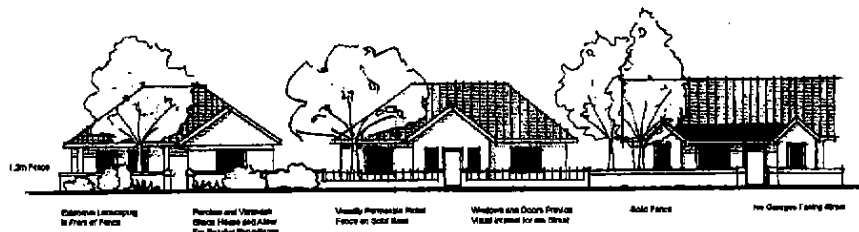
5.15.1 No lot shall be resubdivided or developed to a greater density than R20.

5.16 Variation

5.16.1 Development in accordance with these planning design controls is deemed to comply. However, alternative designs may be considered subject to demonstration that the proposed development is in keeping with the objectives and intent of the planning design controls and is subject to Council approval.



TYPICAL STREET ELEVATION - ROSEBERRY LANE & SHEFFIELD CLOSE



TYPICAL STREET ELEVATION - KINGSTON DRIVE

EXPLANATORY NOTES FOR THE PLANNING DESIGN CONTROLS - DETAILED AREA PLAN (DAP) FOR STAGE 2B SUBDIVISION – KINGSTON ESTATE

Specific Objectives and Intent of the Planning Design Controls

The objectives for the Planning Design Controls for the DAP are as follows:

1. To prevent the resubdivision of lots;
2. To prohibit direct lot access onto Kingston Drive;
3. To ensure that dwellings front onto Kingston Drive;
4. To guide the design and appearance of development (sheds, outbuildings, carports, garages and fencing) along the rear and side laneways for lots with dual or triple road frontage on Kingston Drive. The guidelines promote a built form that contributes to the streetscape with an "active" interface (ie. no blank walls) and visual amenity of the area;
5. To control the height of fencing; for dual and triple road frontage lots on Kingston Drive;
6. To fulfil the conditions of the WAPC subdivision approval;
7. To locate living areas to the rear of dual and triple frontage lots on Kingston Drive; and
8. To ensure all service areas and storage of goods are appropriately screened so as not to detract from the visual amenity of the area.

The majority of the planning design controls are applicable to those lots that have dual and triple road frontages on Kingston Drive, which is consistent with the conditions of subdivision approval. The following justification/explanation is provided for each of the planning design controls.

The following explanatory notes are relevant to dual or triple road frontage lots on Kingston Drive:

Building Orientation and Rear Outdoor Living Areas

Lots with frontages on Kingston Drive shall orientate dwellings towards this road and not to the rear or side laneways. Subsequently, the main outdoor living areas for these lots shall be oriented towards the rear laneways. This will provide a consistent built form along Kingston Drive, contributing to its streetscape. Kingston Drive performs an important role within the Kingston Estate as the key entry and access road. This section of Kingston Drive adjoins the central wetland POS and as such building orientation and built form for highly exposed lots in Stage 2B is critical to the amenity of the area.

Regular Front Setbacks

In order to create and maintain an orderly neighbourhood, front setback areas are an integral part of the streetscape and fundamental to the character of the Estate. The planning design controls require a regular dwelling setback of 6 metres (with no averaging) for lots with frontage to Kingston Drive. This requirement is consistent with the R Codes.

Access for Kingston Drive Lots

Kingston Drive is the key access road for the entire Kingston Estate. Due to the large number of residential lots within the Estate, Kingston Drive will carry a high level of traffic. Accordingly, it is recommended in the planning design controls that no direct lot vehicular access be permitted onto Kingston Drive. Instead vehicular access to dual and triple road frontage lots shall be provided from the rear laneways of Rosebery Lane and Sheffield Close. Therefore, garages and carports shall not be permitted along Kingston Drive and side laneways (ie. Norfolk Lane and Brighton Way) due to the proximity of a roundabout.

In accordance with Condition 1 of the subdivision approval, restrictive covenants shall be placed on the Certificates of Title of dual and triple road frontage lots to control access onto Kingston Drive. The same requirement is imposed in Condition 20 of the subdivision approval.

Garages and Carports

The planning design controls include a number of requirements controlling the development of carports and garages to ensure that these structures do not dominate the streetscapes of the rear and side laneways to the detriment of the area. It is imperative that the amenity of dwellings located on the opposite side of the rear laneways is not jeopardised by the development of lots with dual and triple road frontages.

On this basis, the planning design controls require that garages and carports are setback at least 1.5m from rear laneway boundaries. This requirement is consistent with the R Codes for building setbacks on secondary roads.

The width/length of garages and carports where they front onto the rear laneways will be restricted to 7m (including walls) to further ensure that these structures do not dominate the streetscape of the rear laneways.

Sheds and Outbuildings

The planning design controls include a 1.5m setback provision for sheds and outbuildings from rear and side laneways for similar reasons to the setback of garages and carports. Again it is intended that these structures do not dominate the streetscapes of the rear and side laneways. Sheds and outbuildings will also be required to be screened by fencing. The controls promote the integration of sheds and outbuildings within garages to minimise the building impact on laneway streetscapes.

Parapet Walls

The planning design controls allow for parapet walls on alternate common boundaries of dual and triple road frontage lots as a way of consolidating the impact of garages, sheds and outbuildings on the laneway streetscapes as opposed to spreading the impact. The DAP identifies the boundaries where these structures can be developed with nil side setbacks.

Minimum Building Width along Rear Laneway Boundaries

To reduce the combined impact of garages/carports and sheds/outbuildings on the rear laneway streetscapes, the planning design controls restrict the maximum portion of the lot width that can be taken up with buildings to 50%. The only exception to this is Lot 608, which has a restriction of 70%, recognising the relatively narrow width of the lot frontage (ie. approximately 10m).

Building Appearance to Rear and Side Laneways

A number of planning design controls are included to control the development of garages, carports, sheds, outbuildings and fencing along rear and side laneways to reduce the building bulk and impact on the laneways and to create an "active" interface. In particular, various treatments are required to be incorporated into the design of these structures to ensure that there are no blank walls. The typical street elevations show various treatments and roof lines that can be used to add "interest" to the buildings located along the rear and side laneways.

To maximise the quality of the built form, no building structures along rear and side laneways will be allowed to have flat roofs, instead the roofline is required to be compatible with the dwelling on the respective lot. Similarly, building materials to be used for sheds, outbuildings, garages and carports

are required to be compatible with the dwelling on the respective lot, ensuring that there is an acceptable level of development that contributes to the streetscape.

Fencing

To further enhance the streetscapes of side and rear laneways and Kingston Drive, fencing on road boundaries for dual and triple road frontage lots will be controlled. The planning design controls for fencing are generally consistent with the R Codes. Fencing along Kingston Drive is restricted to a height of 1.2m to preserve the amenity of the area and to encourage street surveillance.

The fencing controls also address the rear and side laneways for triple and dual road frontage lots. In particular, the controls require that substantial landscaping be undertaken adjacent to fence lines or alternatively landscaping be incorporated in the fence design to "soften" its impact on the laneway streetscapes. Generally, fences will be required to be setback 1.5m from the rear and side laneway boundaries, although this may be reduced where landscaping is incorporated into the fence design (ie. planter boxes or creepers etc).

Fencing along the side and rear laneways are able to be constructed to a maximum height of 1.8m to provide a reasonable level of privacy to outdoor living areas. However, fencing above 1.2m are required to be constructed with permeable materials that will also "soften" their impact on laneway streetscapes.

Services

Any services (ie. drying area, bin storage area) or storage of goods are required to be screened in order to protect the visual amenity of the area, particularly for dwellings located directly opposite triple and dual road frontage lots.

The following explanatory notes are relevant to Lots 630 to 632:

Building orientation

Lots 630 to 632 will be required to have dwellings orientated towards the Leshenault Recreation Reserve. This requirement is consistent with development of lots directly to the north within Stage 2A. In addition, the design of sheds, outbuildings, garages and carports on these lots will need to consider and contribute to the streetscape of Rosebery Lane.

The following explanatory notes are relevant to Lots 613 to 635:

Parapet Walls

The planning design controls allow for parapet walls on alternate common boundaries of lots as a way of consolidating the impact of garages on the laneway streetscapes as opposed to spreading the impact. The DAP identifies the boundaries where these structures can be developed with nil side setbacks. The requirement for parapet walls varies slightly for Lots 630 to 632 where sheds and outbuildings (in addition to garages) can be erected on the parapet wall boundaries due to the required orientation of the dwellings (ie. facing Leshenault Recreation Reserve). The design controls also address the specific parapet wall requirements for Lots 626 and 634.

The following explanatory notes are relevant to all lots in the DAP:

Resubdivision

The DAP restricts any opportunity for further subdivision of lots within Stage 2B of Kingston Estate. At present the land is coded R25, which allows for larger lots (ie 700m² or greater) to be resubdivided. However, the land is in the process of being down coded to R20 through the ODP process, which will resolve this issue. In the interim, the DAP prohibits any resubdivision.

Variation

Whilst the planning design controls are intended to provide requirements to control the built form for lots included in the DAP, they are not meant to be unnecessarily prohibitive. For this reason a standard variation clause has been included to allow Council to consider the merits of alternative design proposals in terms compliance with the overriding objectives and intent of the planning design controls.